

NOTE: This Chapter should not be read in isolation. You may need to consider other chapters of this DCP when preparing your application.



CHAPTER S6: TOWN OF MILTON

Chapter S6: Town of Milton

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Amendment history			
Version Number	Date Adopted by Council	Commencement Date	Amendment Type
1	14 October 2014	22 October 2014	New
2	1 December 2015	23 December 2015	Amendment

1 Purpose

The purpose of this Chapter is to provide guidelines to assist applicants and to identify matters being assessed by Council when considering development applications for Milton and surrounds. The DCP also provides guidance to Council with respect to overall directions for public domain improvements.

The town centre contains several significant heritage buildings and places, and combined with a relatively large commercial and retail centre and sub-regional hospital, Milton is a commercial and community destination for many surrounding rural and coastal communities.

Advisory Note: In addition to the provisions outlined in this Chapter, you must refer to Supporting Map 1 – Indicative Town Centre Master Plan and Supporting Document 1 – Urban Design Companion.

2 Application

This chapter applies to the area known as the Milton urban area and immediate surrounds, as shown in Figure 1 below.

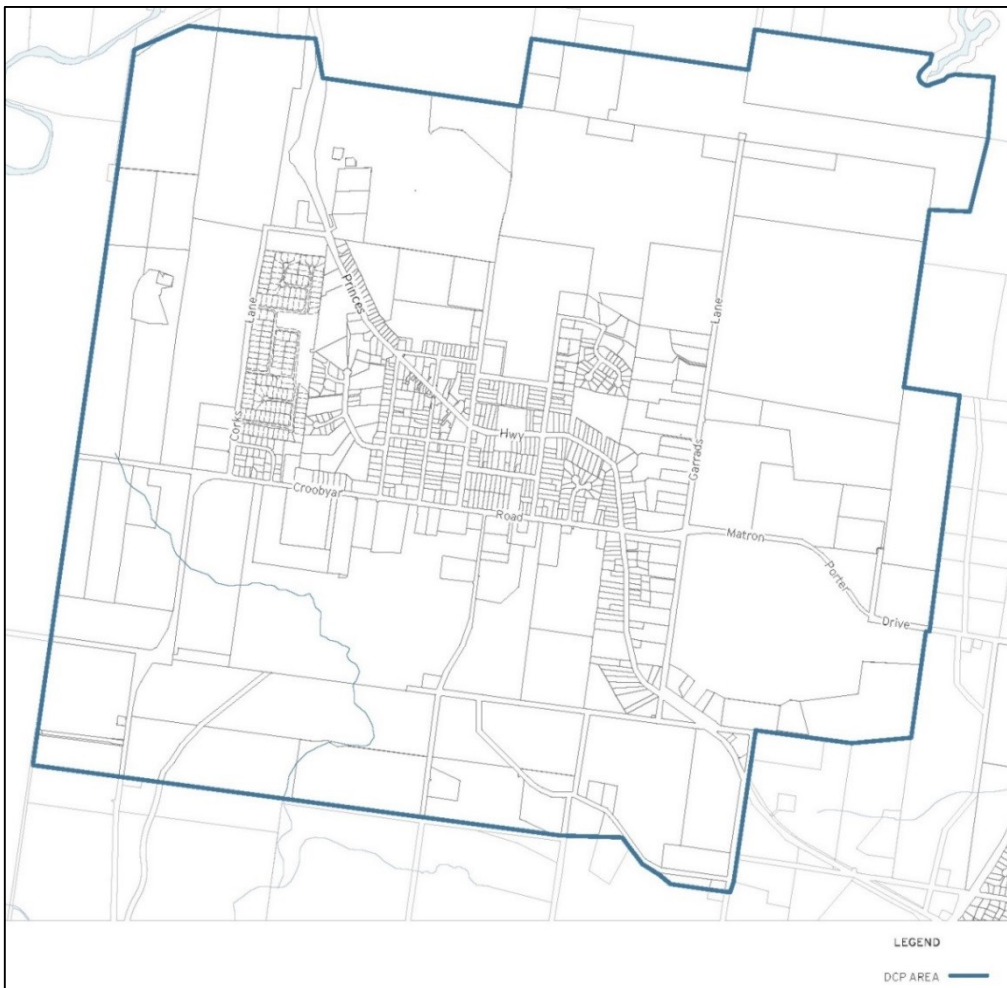


Figure 1: Subject Land Map

3 Context

Milton is memorable for its rural, historic, village, and community character. Whilst close to larger urban areas, it retains a village atmosphere where its smaller scale buildings and defined boundaries give way to rolling rural farmland that meets the coastline beyond. Combined with its stunning views, Milton is an eclectic place full of local boutique experiences.

The future of Milton will see appropriate growth that retains this character, complements its history and heritage, and brings opportunities for all ages - from local retail, health and aged care services, to the retention of the agricultural lands that defines the towns' edges for future generations to experience and utilise.

3.1 Existing Character

Milton has a well-established and recognised character primarily resulting from the unique topography, distinct rural interface and the high number of heritage buildings developed from around 1860. The character of the town can be defined in more detail through four key precincts summarised below and identified in Figure 2:

- Town Centre Precinct: contains a high number of significant heritage buildings in close proximity to the Princes Highway. One or two storeys predominate with views to the coast and mountains from either side of the main ridgeline/highway corridor. High traffic levels and associated parking are ever present, while small, eclectic retail experiences are prominent in the town centre retail setting, with pedestrian thoroughfares providing for “hidden” opportunities.
- Established Residential Precinct: expands from the town centre, with high quality heritage character and architectural detailing on numerous original dwellings. Grid streets traverse downwards from the ridgeline with significant views. Dwellings on streets running down slopes are typically one storey with a number of two storey dwellings on streets running across slopes particularly on the high side. Some diversity of use exists through home businesses and tourist accommodation. Streets have an open feel, with dwellings addressing the street with carports and garages typically to the rear of allotments. The Precinct also contains a sub-regional hospital which has evolved into a consolidated area of health professionals and consulting rooms - referred to as the Health and Hospital Sub-Precinct. This area has retained and adapted many original dwellings, whilst the hospital complements this scale and design features.
- Emerging Residential Precinct: generally contains more recent residential development with a range of architectural quality and mixed contribution to the overall town character. The precinct is broken into a number of areas, none of which have visual prominence from the primary approaches along the Princes Highway. Architectural styles vary widely between and have limited consistency.
- Rural Surrounds Precinct: includes areas that are highly visible from the town and which provides a notable characteristic of Milton and its rural history. Farms are predominantly associated with the dairy industry, many being heritage listed. The farms are characterised by rolling green hills, basic farm buildings utilising timber and corrugated iron as key materials, and by landscape features including landscape plantings for wind breaks and along roads. The precinct also includes a range of non-agricultural uses, such as schools, a small industrial area and approved but not constructed aged care facilities. These have varying character traits but fall outside the defined residential boundaries of the town and are often contiguous to the rural surrounds.

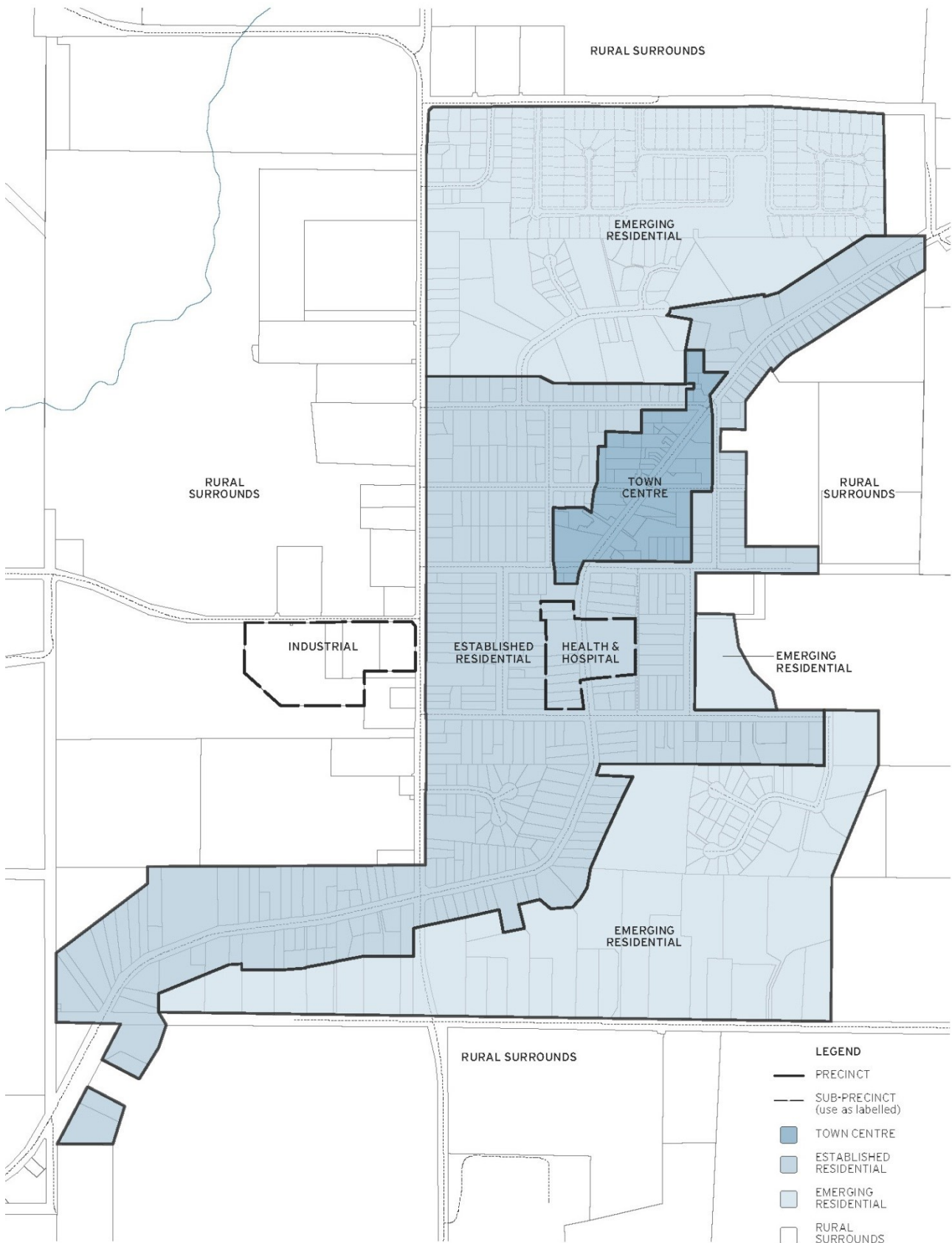


Figure 2: Precincts

3.2 Desired Future Character

The desired future character of Milton will seek to retain the key elements of the overall village feel predominately associated with:

- Being small in scale;
- Retaining heritage and other contributory buildings;
- Maintaining key landscape features and views to rural surrounds;
- Maintaining separation from other urban areas to the south and east; and
- Maintaining the intricate and unique range of retail experiences.

Within each precinct, the following summary of the desired future character and objectives are provided:

3.2.1 Town Centre Precinct

Desired Future Character Statement: Continue to facilitate the existing unique attributes which make Milton a popular place to live and visit. Concentrate on vacant and in-fill development to create a secondary orientation away from the Princes Highway to capture views and activate off-street parking areas. Building lines will reinforce and define town entrances, with improved pedestrian accessibility and comfort being key future directions. The retail character will continue to create a unique shopping experience through small, independent and high quality outlets. Larger internalised “shopping centre” type development would be discouraged.

Town Centre Precinct Objectives:

- i. To retain the prominent built form along the Princes Highway, with a maximum height of two storeys along the Princes Highway frontage and stepping down to lower levels for new buildings further from the highway and thereby retaining longer distance views from the ridgeline;
- ii. To allow for expansion of the town centre through infill development and to activate existing off-street car parks to the east and west of the Princes Highway;
- iii. To retain a fine grain architectural style and patterns of use that have resulted in the mix of small, unique and interesting retails experiences;
- iv. To provide improved connectivity to existing infrastructure such as off-street car parks, the Village Green and the swimming pool through clearer pedestrian connections; and
- v. To improve definition of Princes Highway crossing points through the creation of nodal points resulting from streetscape improvements and complementary built form on adjoining land.

3.2.2 Established Residential Precincts

Desired Future Character Statement: Maintain existing character through careful consideration and interpretation of the key built form elements of the area including:

- Typically single storey building scale;
- Low height and permeable fencing;
- Utilising setbacks that are consistent with the predominant setbacks in the immediate locality;
- Utilising materials and colours that are consistent with the area; and
- Wherever possible, that carports and garages are located to the side or rear of properties.

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Intensification would be possible, but designed so as to not compromise the built form and character of the area.

Established Residential Precinct Objectives:

- i. To retain the built form and character traits of established areas in any new dwellings or modification of existing; and
- ii. To encourage home occupations and other permissible non-residential uses that promote the cottage industry feel of the area and which creates small scale employment opportunities.

3.2.3 Emerging Residential Precinct

Desired Future Character Statement: Newer residential areas should not be forced to mimic established areas, but to establish their own contemporary character based on more basic controls to ensure their overall scale does not detract from the views and character of existing areas. Materiality controls for established areas would guide emerging residential area trends to ensure some continuity across the town.

Emerging Residential Precinct Objectives:

- i. To provide contemporary residential areas that reflect the build form scale of the broader Milton area but do not mimic the original more established residential areas; and
- ii. To avoid new residential development becoming prominent from longer distance views to Milton, or from views currently enjoyed by existing urban areas of the town.

3.2.4 Rural Surrounds Precinct

Desired Future Character Statement: On areas zoned for rural purposes, continue to maintain the core elements of the existing character being open, productive and well defined through built form and landscape treatments. Use of existing prominent materials of rural development would continue, while productive agricultural land would be retained in the long-term for food production. On land zoned for other purposes, such as the small industrial area, and for development with substantial built form such as schools or aged care, proposals will be low and open in scale as far as practical, whilst still seeking to utilise materials and styles that are consistent with the prevailing rural context and incorporating landscape that softens views to the site.

Rural Surrounds Precinct Objectives:

- i. To maintain the open rural views from existing urban areas;
- ii. To emphasise the existing built form and character for new development; and
- iii. To allow for other forms of permissible development that provide an architectural intent that responds to the rural setting and utilises techniques to soften the visual impact through retaining a small scale and incorporating appropriate landscape.

4 Objectives

The objectives are to:

- i. Enable Milton to grow in a controlled and appropriate way;
- ii. Ensure that new town centre development contributes to maintaining the small, rural and unique experiences that are valued by existing residents and visitors;
- iii. Ensure that new development respects the heritage character of the town, seeking to complement and not compete with identified and potential heritage items;
- iv. Enable established residential areas to retain their existing character values through new development having complementary design elements;
- v. Ensure that emerging residential areas are sympathetic to the character of the existing town through being of an appropriate scale and style, whilst establishing a more contemporary character;
- vi. Protect the landscape setting of rural lands;
- vii. Protect views and ensure that they form an important consideration for development, including views to, from and past the subject property, and incorporating the principles of view sharing;
- viii. Encourage built form and landscaping that provides a transition to the surrounding rural landscape; and
- ix. Ensure that Milton is well connected through foot and cycle pathways, as well as through pedestrian access thoroughfares and lanes within the town centre.

5 Controls

5.1 Public Domain

Public domain areas are to form a consistent component of each development to ensure consistency and connection between the built form and public space. Within the Town Centre Precinct and on key access routes, the public domain would link to other publically accessible areas, even through private land where such linkages are available (or otherwise enabling for future allowance for such linkages where not available).

Improved pedestrian circulation around the town centre and adjoining precincts is also important to the commercial success of the village centre and in meeting the desire of visitors and locals to explore the area in relative comfort. Access across the highway and between key areas such as off-street parking and the Village Green would be improved through public domain works, encouraging an enlarged central business area.

Figure 3 and Figure 4 identify the extent of proposed public domain works in the town centre area. Parts of these works have already been completed whilst others are dependent on Council funding or private development. It is noted that some areas of private land are included in these plans as being conceptual locations for public domain improvements corresponding with the Indicative Town Centre Master Plan (Supporting Map 1). Development of these areas shall be subject to agreement with the relevant owners and Council.

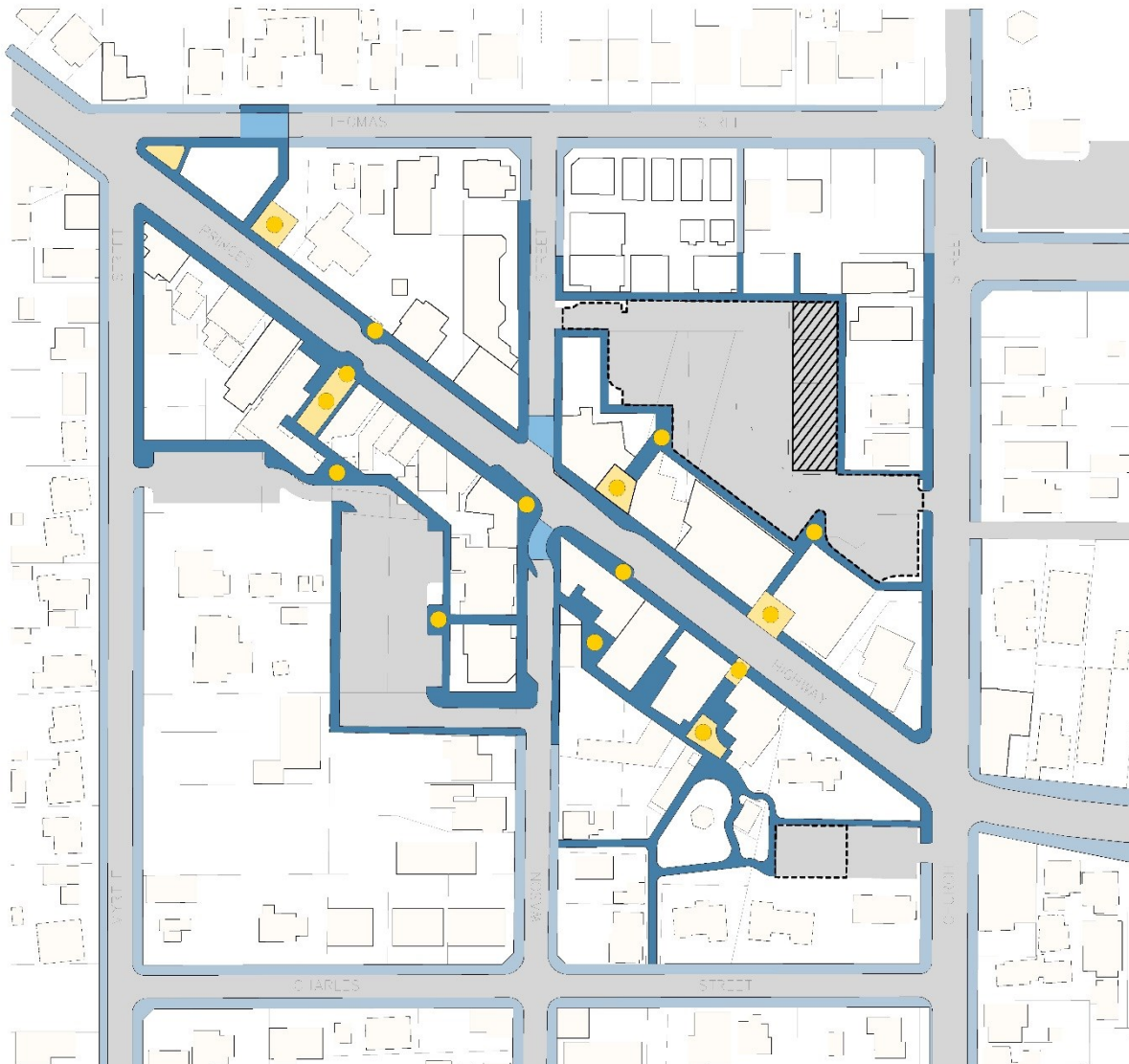
5.1.1 Objectives

- i. To strengthen pedestrian accessibility and comfort along the Princess Highway corridor through:
 - a. Emphasis on town centre gateways at the intersection of Myrtle/Thomas street and the Princes Highway in the north-west and Church Street and the Princes Highway in the south-east;
 - b. Increase in the size of pedestrian pathways and introducing pedestrian refuge build-outs (kerb extensions) on the northern side of the intersection of Wason Street and the Princes Highway;
 - c. Reinforcing existing pedestrian crossing points adjoining the Post office in the north and near the Wason Street/Princes Highway intersection;
 - d. Creating new crossing points near the entry to the Village Green and at the intersection of Myrtle Street and the Princess Highway; and
 - e. Introduction of new landmark trees and awnings to be integrated with dining and other street activity to provide shade and weather protection and to compliment the village heritage and character.
- ii. To strengthen pedestrian links to adjoining established residential areas through footpaths between the Princess Highway and Croobyar Road along Myrtle, Wason and Church Streets, and between Thomas Street and the Princess Highway along Church Street;
- iii. To provide a smaller number, but more well defined pedestrian thoroughfares between off-street parking areas and the Princes Highway and other street frontages;
- iv. To provide linkages along the Princes Highway, including the Health and Hospital Sub-Precinct, to facilitate pedestrian movement along the main ridge between residential and commercial areas;
- v. To establish shared paths along major road linkages between Milton and other centres including along Croobyar Road, Princes Highway and Matron Porter Road; and
- vi. To enhance pedestrian amenity in the Town Centre Precinct with public art, signage and other sculptural elements to reinforce activity nodes in key areas.

The overall intent of these objectives is highlighted in the Indicative Town Centre Master Plan (Supporting Map 1), as well as the DCP area plans for Access & Circulation and Vegetation Works, contained in the Supporting Document 1 - Urban Design Companion to this DCP. Public Domain Hardworks and Vegetation plans for the town centre area are also provided in Figure 3 and Figure 4 respectively. It is reiterated that some areas of private land are included in these plans as being conceptual locations for public domain improvements corresponding with the Indicative Town Centre Master Plan (Supporting Map 1). Development of these areas shall be subject to agreement with the relevant owners and Council.

5.1.2 Performance criteria and acceptable solutions

Performance Criteria		Acceptable Solutions	
P1	Provide pedestrian connections to connect public spaces through and around private development and to facilitate the delivery of the overall public domain plan in the Town Centre Precinct	A1.1	Pedestrian connections are made in accordance with the <i>Public Domain Hardworks</i> plan at Figure 3 where applicable; and
		A1.2	Where existing pedestrian connections have been allowed for or approved on a neighbouring or nearby site, proposed development shall ensure compatibility with these connections.
P2	Use of materials is consistent throughout areas of public accessibility	A2.1	Materials used are consistent with the palette provided as part of the <i>Public Domain Hardworks</i> plan at Figure 3; and
		A2.2	Vegetation and planting shall be designed consistent with the location and planting palette provided in the <i>Public Domain Vegetation</i> plan at Figure 4.



FURNITURE SCHEDULE

General:	All steel finishes on furniture to be powder coated in Taubmans 'Claret JG142'
Bollard:	Street Furniture Australia 'B2 Bollard'
Tree Guard:	Street Furniture Australia 'TG4 Tree Guard'
Bin:	Custom SCC Design
Seat:	Street Furniture Australia 'CMP1 2100mm'

LEGEND

- PAVEMENT TYPE 1**
 Feature pavement rugs at nodal points. Herringbone brick paving with soldier header course to match existing 'P.G.H. Sovereign Gold'. Includes feature concrete seating located in informal groupings around mosaics.
- PAVEMENT TYPE 2**
 General town centre streetscape treatment. Herringbone brick paving with soldier header course to match existing 'P.G.H. Black & Tan'. Includes a palette of off the shelf street furniture. refer Furniture Schedule for details.
- PAVEMENT TYPE 3**
 Pigmented & stamped bitumen in herringbone paver pattern at pedestrian crossing thresholds.
- PAVEMENT TYPE 4**
 1500mm wide concrete path and grass verge along residential frontages.
- TILE MOSAIC:**
 Public art element at nodal points. Local themes by local artists to celebrate Milton's boutique and art qualities. To compliment and extend on existing mosaics in streetscape.
- FUTURE CAR PARK**
- FUTURE PRIVATE CAR PARKING**

Figure 3: Public Domain Hardworks Plan – Town Centre



- LEGEND**
-  **Landmark Trees:**
Araucaria bidwillii, Araucaria heterophylla, Ulmus parvifolia, & Ficus macrophylla.
 -  **Standard Street Tree:**
Callistemon viminalis, Cuppaniopsis anarcardiodes, Tristainiopsis laurina,
 -  **Town Centre Highway Tree:**
Crepe Myrtle, Magnolia sallangeana
 -  **Shrub and Understorey Planting**
 -  **Turf Areas**
 -  **Future Car Park**
 -  **Future Private Car Parking**

Figure 4: Public Domain Vegetation Plan – Town Centre

5.2 Activities

The diversity of uses and activities occurring in Milton is recognised as one of the positive attributes of the town. The continuation of variety and diversity of the retail, community and food and beverage offerings should be encouraged to the extent of land use permissibility enabled by the applicable zone.

Within the town centre, uses and activities should maintain the vibrant and high quality retail and food and beverage offerings, interspersed with community and cultural buildings and functions. Commercial uses should be located in a second storey or be designed, implemented and operated in way that creates active interaction with the street or other public areas. Identified activity nodes should be reinforced and become “meeting points” where public facilities such as street furniture, information, art, public buildings, spaces and infrastructure are located.

Suitable home occupations, home business and home industries should be encouraged in the established and emerging residential areas.

5.2.1 Objectives

- i. To continue to activate street frontages and other publicly accessible areas through a vibrant mix of retail and food and beverage offerings at the ground level of the Town Centre Precinct;
- ii. To encourage commercial office or similar uses to be located on an upper storey or to otherwise demonstrate activation at the street level;
- iii. To reinforce activity nodes and areas of public use through activation, connectivity and an appropriate level of enclosure through surrounding buildings;
- iv. To ensure buildings are designed to enable a mix of uses to adequately cater for changing trends and activities; and
- v. To encourage small scale home occupations, home business and home industries that are compatible with surrounding residential areas.

5.2.2 Performance criteria and acceptable solutions

Performance Criteria		Acceptable Solutions	
P3	Active uses provided to street frontages, public spaces and core activation area wherever possible in the Town Centre Precinct	A3.1	The ground level of any development in the town centre, identified for “street activation” in the Street Activation plan at Figure 5, is to be open to the street or publicly accessible areas (including thoroughfares) to encourage activation, with minimal signage, or other impediments to visual connections between internal and external spaces; and
		A3.2	Ground level to be predominately or completely occupied by retail, food and beverage or community uses; and
		A3.3	Where commercial office or other uses are proposed at the ground floor, such uses are to provide frontages which provide demonstrated interest and activation to the street in other ways, such as reception areas being visible and open to the street.

Performance Criteria

Acceptable Solutions

P4 Provide for home occupations, home business and home industries in residential areas

A4.1 Home activities and businesses are undertaken consistent with Chapter G19 Home Based Business Activities and *State Environmental Planning Policy 2008 (Exempt and Complying Codes) 2008*.

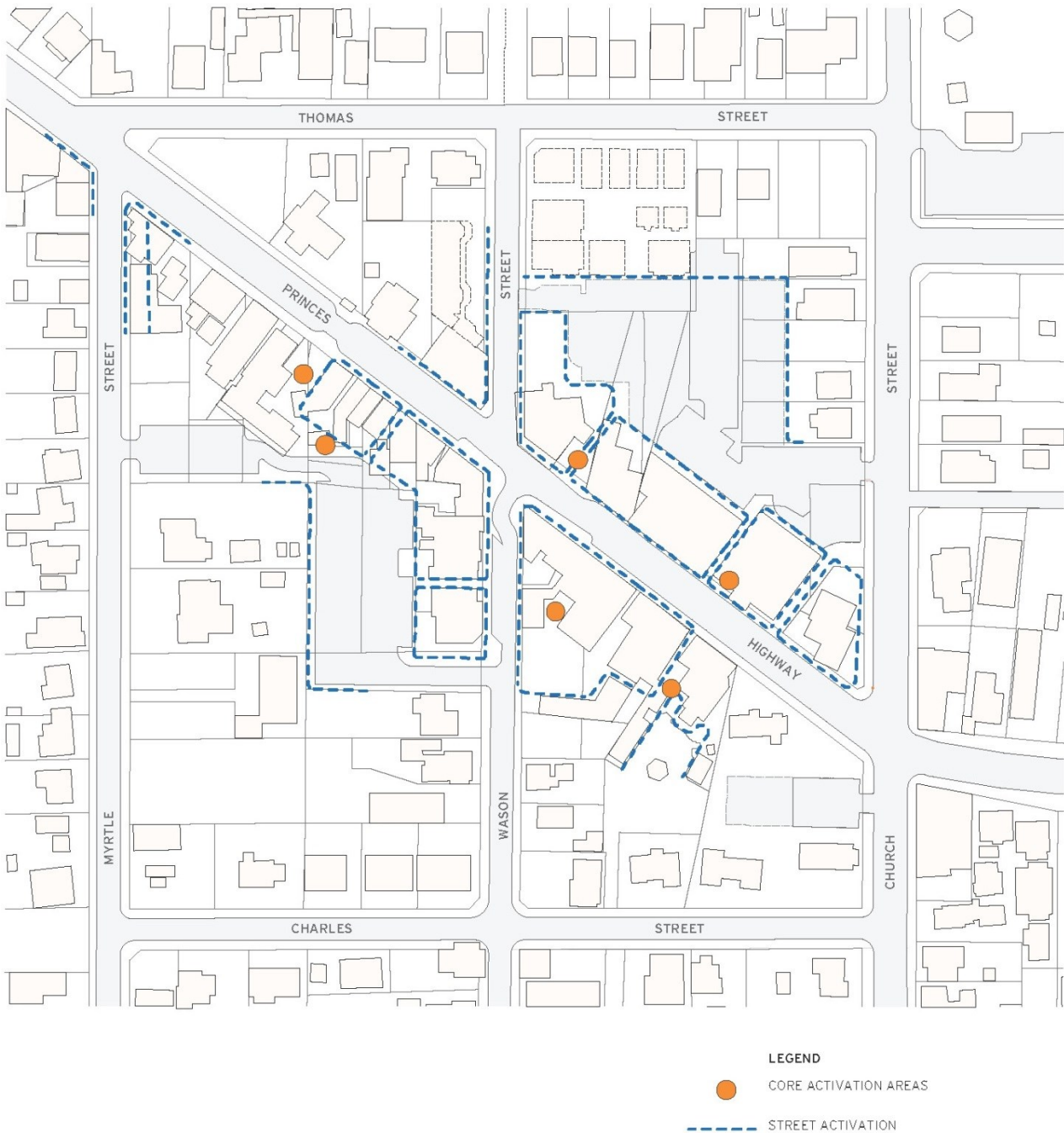


Figure 5: Street Activation

5.3 Views

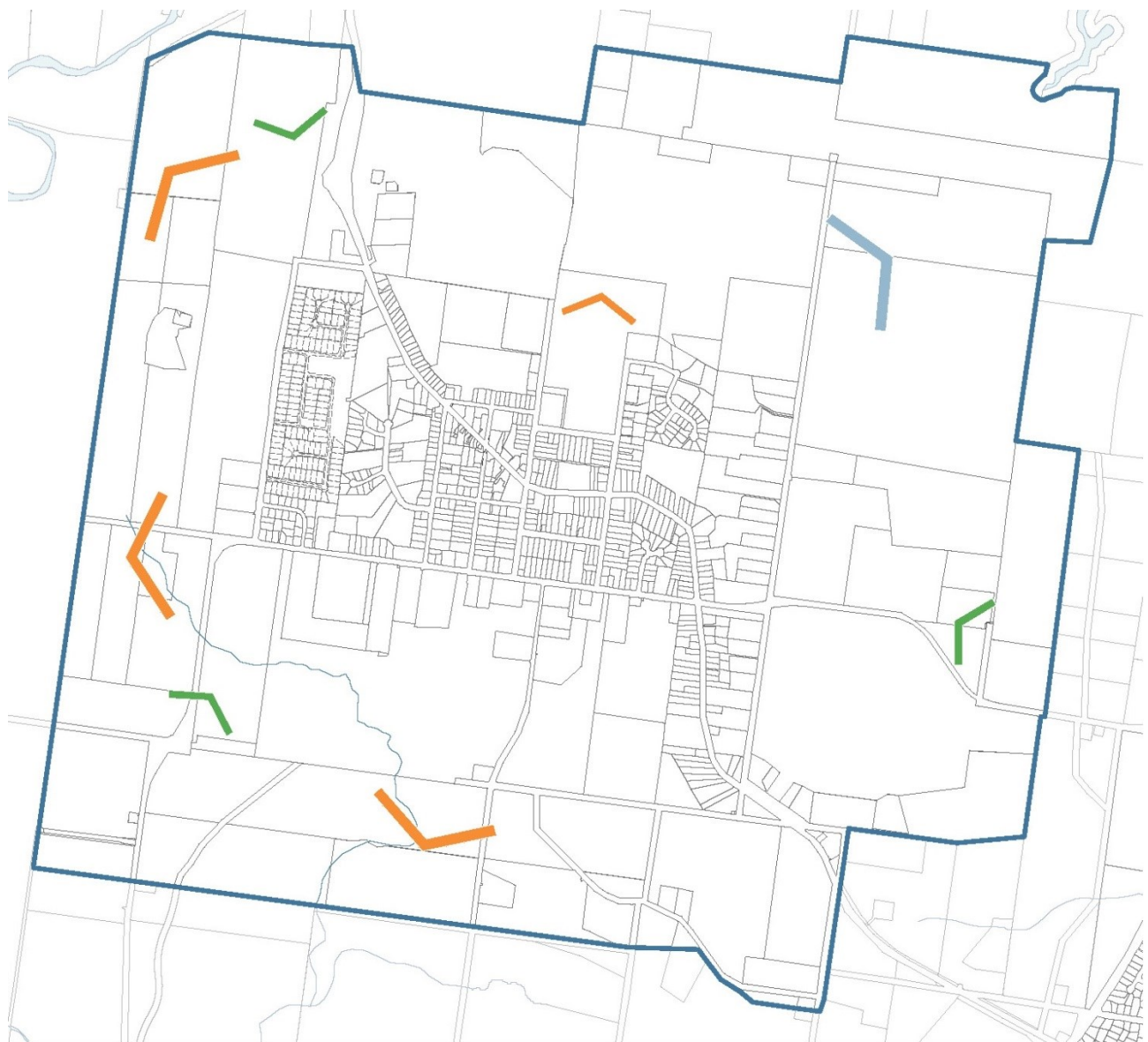
The underlying topography of the Milton area provides for a unique opportunity to facilitate views from the town centre to the ocean to the east, and views to the iconic Pigeon House Mountain and ranges to the west. Key views exist from the Princes Highway through existing road reserves, between existing buildings and from the rear of commercial properties with frontages to the Princes Highway or at higher levels of the prevailing topography. Retention of these views will ensure the ongoing connection of town centre and residential areas to its rural surrounds.

5.3.1 Objectives

- i. To provide generous setbacks to corners and along street where views can be maintained or enhanced along existing road reserves;
- ii. To encourage new development to utilise existing or potential views through private land;
- iii. To avoid the blocking of views from existing development, and to embrace the principles of view sharing; and
- iv. To give consideration to the impact of streetscape elements such as street trees and furniture on existing views.

5.3.2 Performance criteria and acceptable solutions

Performance Criteria		Acceptable Solutions	
P5	Retain significant short and longer distance views from the town to surrounding areas	A5.1	Buildings and other structures are designed to maintain the views as identified in <i>Town Centre Views</i> or <i>DCP Area Views</i> plans at Figure 6 and Figure 7; or
		A5.2	Buildings and other structures in the Town Centre Precinct are designed to frame views as described in <i>Town Centre Views</i> plan at Figure 6 and to utilise them as part of the design intent; and
		A5.3	Potential views identified in <i>Town Centre Views</i> plan at Figure 6 are promoted through careful site planning and building design to enable new view opportunities to be created and reinforced.
P6	Respect views from existing development in the design and implementation of new development	A6.1	New development makes reasonable allowance of view sharing through application of the NSW Land and Environment Court's Planning Principles relating to view sharing.
P7	Ensure development is not visually intrusive when viewed from approaches to the town	A7.1	Development proposals demonstrate consistency with prevailing building heights when viewed from "Rural View" locations identified in the Broad Views plan at Figure 7, as well as from the intersection of Princes Highway and Little Forest Road to the north.



LEGEND

RURAL VIEW TO MILTON
Significant rural character when
approaching Milton



SEA VIEW
Vista to sea and Green island



RURAL / MOUNTAIN VIEW
Escarpment view with rural
foreground



DCP AREA



Figure 7: DCP Area Views

5.4 Parking, Access and Circulation

Parking for vehicles is recognised as a major issue in the town centre, with some existing on and off-street parking areas regularly at capacity. Future development in the town centre would contribute to the consolidation of car parking areas into a number of key locations that minimise visual intrusion from the Princes Highway, whilst providing direct pedestrian access to adjoining properties and access points to the Highway and town centre. On-street parking would also continue to be available for short-term and the attraction of passing trade. Parking for bicycles would be located in dedicated areas within the streetscape, and in conjunction with a consistent palette of street furniture.

Access to and around the town centre would be facilitated by clear, consistent and safe thoroughfares and pathways. These would provide separation from traffic and servicing areas where possible and circulation routes around the town centre would be provided at ground and potentially at upper levels through publically accessible verandahs and walkways that link between private sites. Outside the town centre, development known to create a demand for car parking would provide an appropriate level of parking on-site or under an acceptable arrangement to Council and in accordance with relevant policies of Council.

5.4.1 Objectives

- i. To create a compact, accessible and connected town centre where vehicle parking is not visually dominant;
- ii. To locate and provide for consolidated vehicle parking areas that are easily accessible to the town centre via convenient pedestrian thoroughfares;
- iii. To continue to provide for parallel on-street parking, with time limitations at certain times, in the town centre and in other localities where required;
- iv. To ensure development that is outside the town centre provides adequate parking within the development site;
- v. To encourage the use of bicycles through the provision of conveniently located cycle parking areas in key locations; and
- vi. To establish new and reinforce existing shared or dedicated servicing locations for larger retail and commercial operations.

Further details are found in the *Access and Circulation* plan at Figure 8.

5.4.2 Performance criteria and acceptable solutions

Performance Criteria		Acceptable Solutions	
P8	Pedestrian access between the Princes Highway and off-street parking areas is provided to facilitate convenient and comfortable pedestrian movement.	A8.1	Pedestrian thoroughfares are provided in accordance with locations identified in the Access and Circulation plan at Figure 8, and in accordance with other requirements of this DCP; and
		A8.2	Pedestrian thoroughfares are designed with adequate accessibility and activation of the space to ensure safe, direct and efficient access including direct line of sight and minimum width of 3.0m between the proposed

Performance Criteria		Acceptable Solutions	
			building and site boundary where a shared thoroughfare is proposed, or a total width of 5.0m where the thoroughfare is provided entirely within a site
P9	Pedestrian access through private land is provided to facilitate movement along desire lines that are not associated with streets.	A9.1	Pedestrian access is provided in accordance with pedestrian circulation lines as outlined in the Access and Circulation plan at Figure 8 whether at ground or upper levels where possible; or
		A9.2	Alternative pedestrian access arrangements are identified, including potential for future connections, to facilitate movement through private sites in the Town Centre Precinct.
P10	Car parking for future development of the following properties shall be private car parking located in accordance with Figure 8: <ul style="list-style-type: none"> - Lot 6 DP 23970 (No. 29) Church Street; - Lot 7 DP 23970 (No. 31) Church Street; - Lot 8 DP 23970 (No. 35) Church Street; and - Lot 9 DP 23970 (No. 37) Church Street. 	A10.1	Future private car parking design must consider provision of connections between each property.
		A10.2	Future private car parking design considers the provision of a single driveway access from Church Street consistent with Supporting Map 1 – Indicative Town Centre Master Plan.

Advisory Note: Council may require as a condition of development consent that approved and constructed private car parking must be maintained at a standard equal to the approved construction plans for the life of the development.

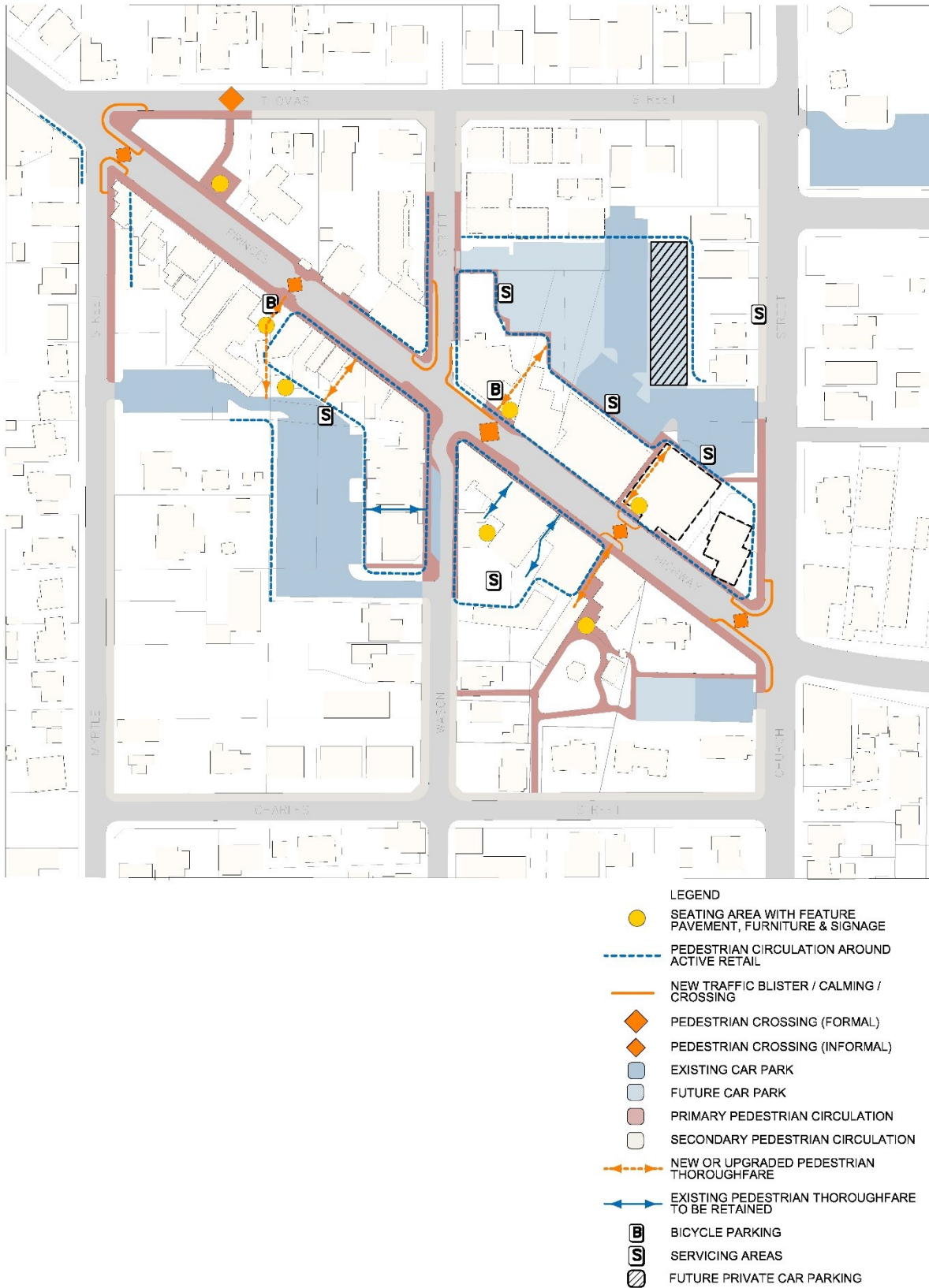


Figure 8: Access & Circulation Plan

5.5 Built Form

The original buildings of Milton comprise of two basic forms, commercial buildings with parapets facing the street and skillion roofs at the rear, and public and domestic buildings usually freestanding, single or two storeys with pitched roofs both hipped and gabled. Outside Milton's commercial spine, residential structures tend to be modest and low-key in style, generally one storey, utilising pitched roofs and having extensive garden areas at the front and/or rear.

5.5.1 Objectives

- i. To maintain the distinction between residential and commercial buildings, providing particular delineation between the town centre commercial core along the Princes Highway and surrounding residential areas;
- ii. To maintain consistency in the built form of commercial buildings in the Town Centre Precinct, accentuating the existing built form attributes including parapets, awnings and windows/doors; and
- iii. To adopt lower building forms which utilise the key attributes of existing development, including roof pitch, fencing and scale in residential areas.

5.5.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
P11 Town Centre Precinct commercial development shall maintain the consistency of built form along the Princes Highway, reducing in scale as it moves away from this central spine to reflect the surrounding residential scale	A10.1 In the town centre, new buildings should be consistent with adjoining development (or the likely future development where this can be demonstrated), matching where practical to the parapet, awning and similar built form attributes; and A10.2 Building height shall be consistent with the controls identified in the Building Height plan at Figure 9.
P12 Design of residential dwellings in the Established Residential Precinct shall be generally consistent with prevailing built form	A11.1 Dwellings shall be predominantly single storey (unless on streets that run across the slope where two storey dwellings are common), with use of dormer windows and smaller scale two storey structures to the rear of dwellings or where integrated with sloping topography; and A11.2 Pitched roofs shall be used to match existing, or the predominant pitch of surrounding properties; and A11.3 Dwellings shall be broken down into small elements, avoiding long and large areas of continuous walls (10m or less), and utilising built form elements such as pavilions with individual roof elements.

Performance Criteria		Acceptable Solutions	
P13	Design of residential dwellings in the Emerging Residential Precinct shall be predominantly single storey to the street frontage and avoid being prominent from distant views	A12.1	Pitched roofs shall be used generally matching surrounding properties, typically between 15 and 30 degrees; and
		A12.2	Second storey of buildings to be set back from the dwelling frontage and utilising the topography where possible.

5.6 Building Height

The height of a building, combined with other attributes such as setbacks and scale, has a direct bearing on how the building is perceived from public spaces such as roads and parks. Within the town centre, the height of buildings plays an important role in enhancing view corridors along the Princes Highway and outward to coastal and mountain views, as well as emphasising definition of the area. Coordinating building height and scale can also give definition to the street character and provide a sense of enclosure that is often desired in town centres. For the Milton town centre area, this is best served by a relatively uniform building height and scale that relates well to other building and provides appropriate definition to the street.

In other areas, building height should be of relatively low, reflect the prevailing character of the area, with single storey dwellings being predominant, some with smaller areas of a secondary storey typically set back from the street. On streets running across the slope, two storeys at the street frontage on the high side is common and may be appropriate for new development. The building height, combined with other building controls, should seek to retain the open character and longer distance views.

5.6.1 Objectives

- i. To provide a graduation of height and scale of commercial buildings in the Town Centre Precinct, with larger buildings along the Princes Highway corridor and reducing gradually as they move away from the ridge line to protect views and integrate with residential scale beyond;
- ii. To enable height of up to three storeys at the rear of buildings where appropriate in the Town Centre Precinct as a result of favourable topographical conditions to enable longer distance views to be captured; and
- iii. To ensure relatively low level buildings in residential areas in keeping with the historical low-key residential development of the locality.

5.6.2 Performance criteria and acceptable solutions

Performance Criteria		Acceptable Solutions	
P14	Height of buildings is consistent with adjoining buildings or desired future character	A13.1	Commercial and retail building heights will generally be limited to one or two storeys with the exception of those areas identified in the Building Height plan at Figure 9, where an additional third storey may be possible set

Performance Criteria	Acceptable Solutions
	<p>back from the road and in keeping with the topography of the locality (also shown in the Cross-Section at Figure 10); and</p> <p>A13.2 Height and/or scale of commercial and retail buildings will reduce as they transition from the Princes Highway to surrounding residential areas; and</p> <p>A13.3 Residential development shall be limited to one or two storeys, with any second storey to be set back from the building frontage; and</p> <p>A13.4 Height of residential development shall otherwise be consistent with the provisions of <i>Chapter G12 - Dwelling Houses, Additions and Ancillary Structures</i> or equivalent Council policy and relevant Local Environmental Plan provisions.</p>

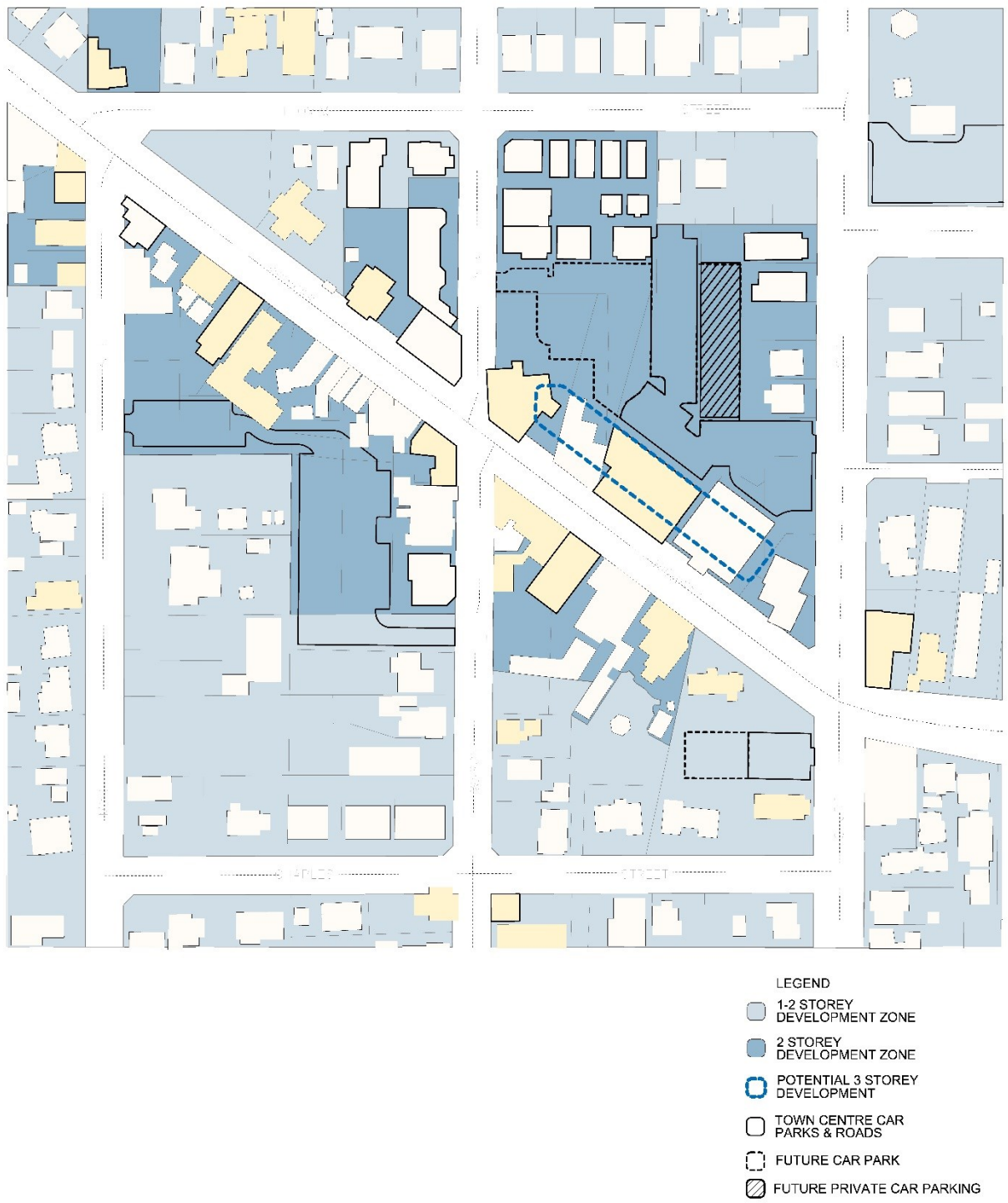
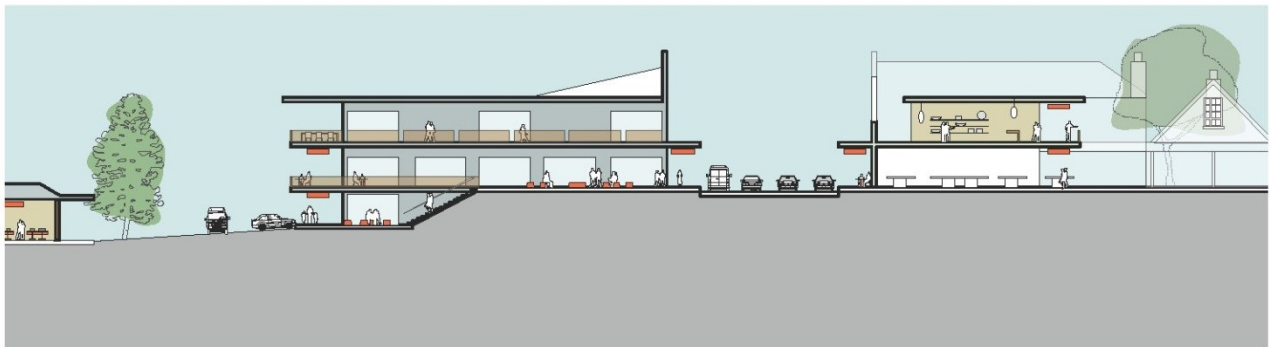


Figure 9: Building Height Plan



CROSS SECTION A - SOUTHERN VIEW THROUGH STAR HOTEL



CROSS SECTION B - SOUTHERN VIEW THROUGH NEW NODE BETWEEN HOTELS



CROSS SECTION C - NORTHERN VIEW THROUGH WESTERN CAR PARK

Figure 10: Town Centre Cross Sections

5.7 Building Setback and Building Lines

Commercial buildings in Milton, particularly in the town centre area, are generally located on the front boundary whereas residential buildings tend to be set back behind front gardens with variable setbacks in established areas. These general trends add to the streetscape character of the respective areas, providing a more intimate and lively main-street character in the town centre where dining spills out onto the footpaths, whilst providing an open low-scale feel to surrounding residential precincts.

5.7.1 Objectives

- i. To enable buildings in the town centre, particularly along the Highway, to generally be built to street boundaries unless otherwise identified;
- ii. To utilise subtle setbacks in the town centre to highlight existing heritage buildings;
- iii. To establish consistent building lines to development which adjoins off-street parking areas; and
- iv. To provide consistency to residential front and side setbacks.

5.7.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
P15 Retail and commercial development in the Town Centre Precinct shall enhance streetscape character and functionality through application of appropriate building setbacks with consideration of heritage, streetscape enhancement or other extenuating circumstances	A14.1 Setbacks in the town centre are consistent with the Setbacks and Building Lines plan at Figure 11.
P16 In the Town Centre Precinct, side setbacks are to make allowances for public thoroughfares	<p>A15.1 Where no pedestrian access or thoroughfare is indicated in the Access and Circulation plan in Figure 8, side setbacks in the town centre should generally be to the site boundary; or</p> <p>A15.2 Where pedestrian access is proposed in the Access and Circulation plan in Figure 8, side setbacks should be consistent with requirements of Section 5.2.2.</p>

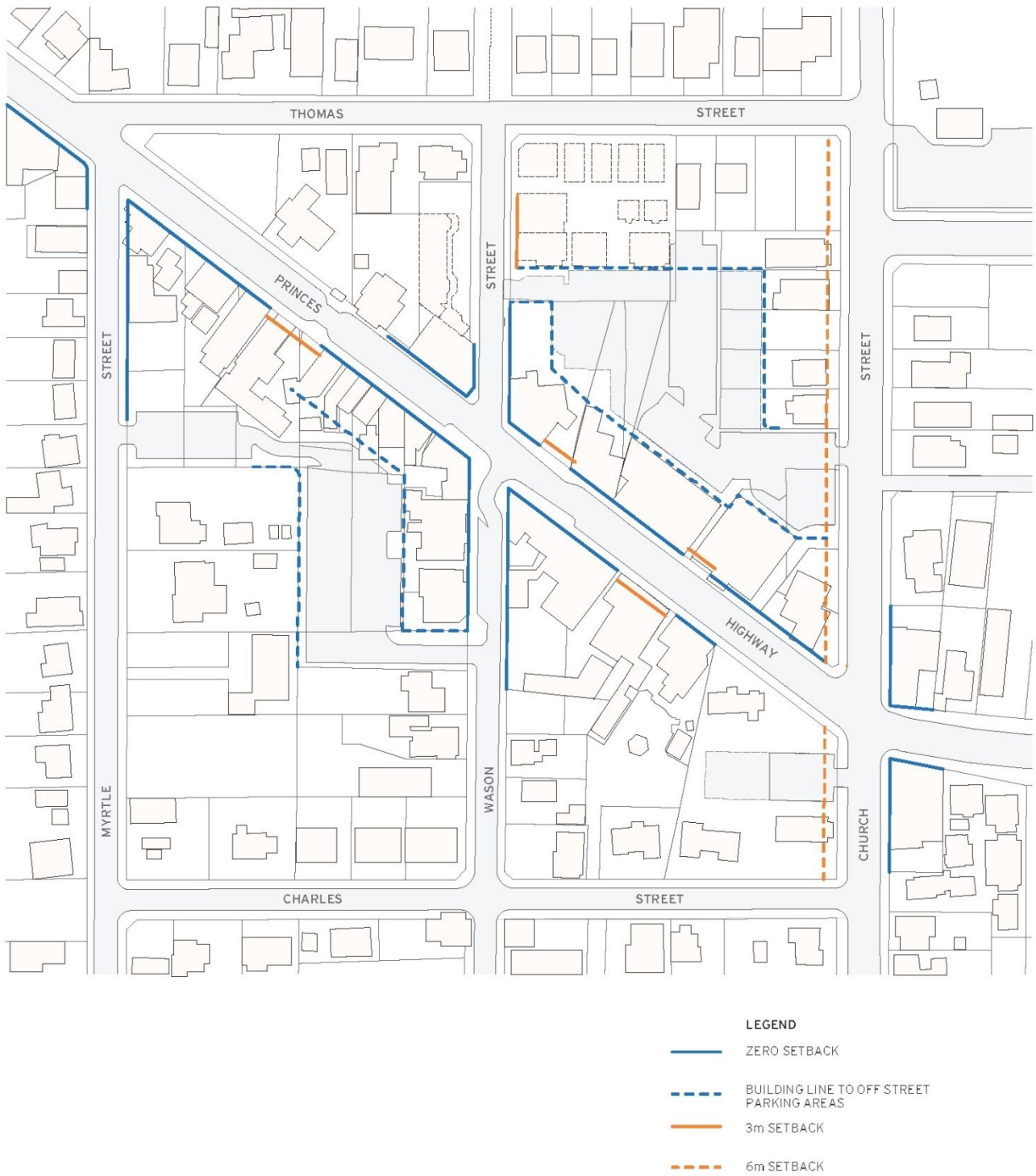


Figure 11: Setbacks & Building Lines Plan

5.8 Contributory Buildings

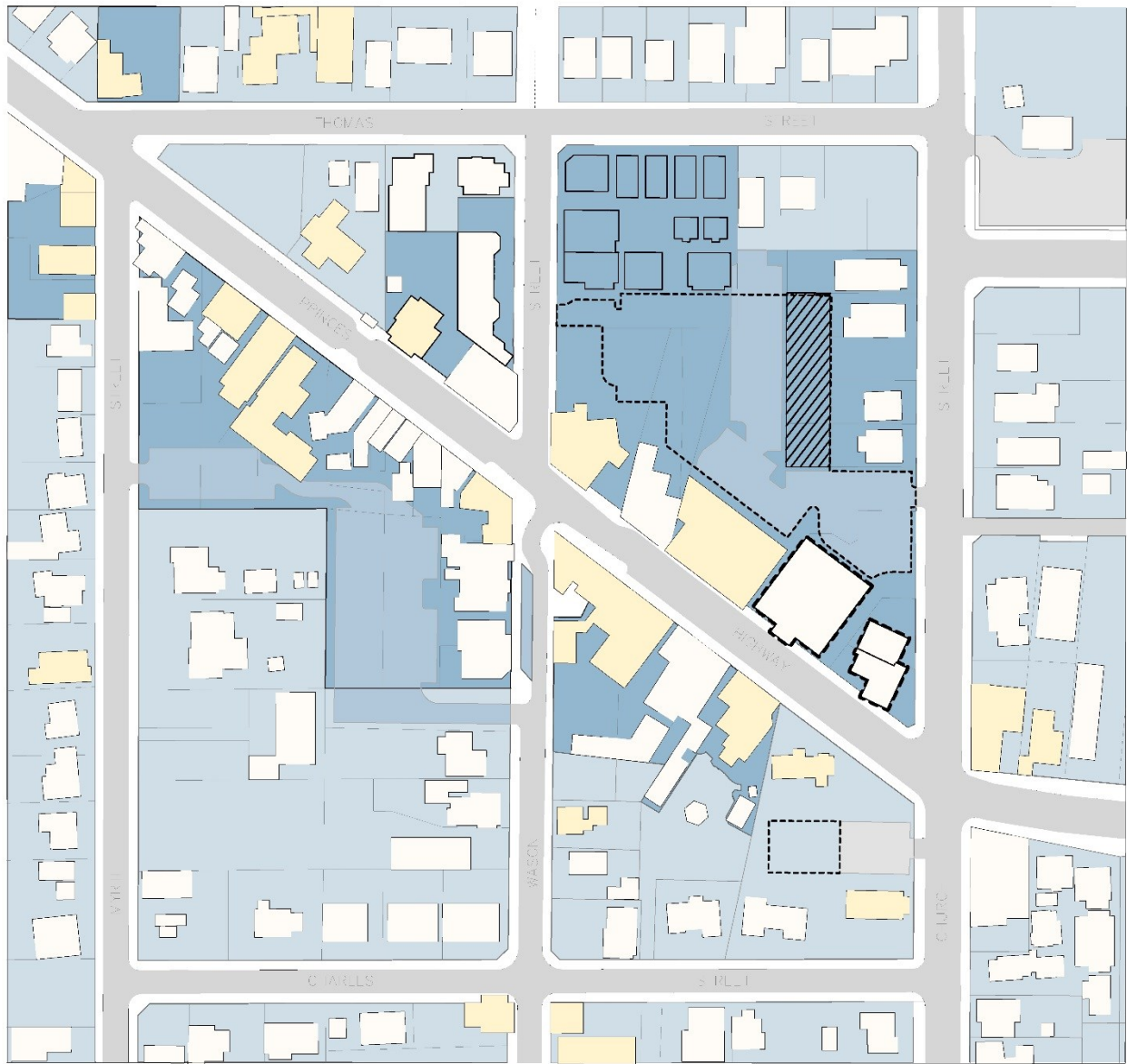
Many buildings within the town centre and established precincts of Milton contribute to the character and amenity of the town that is highly valued by residents and visitors. Buildings that contribute to the character of the area, including heritage and non-heritage listed buildings, should generally be retained, adaptively re-used or integrated into future development should their current use cease to function. This is particularly important in the town centre area.

5.8.1 Objectives

- i. To retain contributory buildings as a core character element of the Town Centre Precinct; and
- ii. To seek the redevelopment or demolition of low visual contribution building rather than prolonging the life of such buildings through substantial upgrades.

5.8.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
P17 Contributory buildings are retained, adaptively re-used or form part of new development	A16.1 Contributory buildings, as identified in the Contributory Buildings Plan at Figure 12, are retained and re-used; or A16.2 Contributory buildings, as identified in the Contributory Buildings Plan at Figure 12, are integrated into future development in a way that maintains the key character elements of the existing building/s.
P18 Low visual contribution buildings are encouraged to be redeveloped to provide a more appropriate streetscape and design response to the locality	A17.1 Redevelopment of sites containing low visual contribution buildings, as identified in the Contributory Buildings Plan at Figure 12, will involve the complete or partial demolition of the existing building and redevelopment to the relevant built form standards of this DCP.



- LEGEND
-  HERITAGE & HIGH CONTRIBUTION BUILDINGS
 -  LOW VISUAL CONTRIBUTION BUILDINGS
 -  EXISTING CAR PARKS AND ROADS
 -  FUTURE CAR PARK
 -  FUTURE PRIVATE CAR PARKING

Figure 12: Contributory Building Plan

5.9 Architectural Character

Whilst the size and scale of a building plays a major role in establishing spaces and relationships, when in closer contact with individual buildings, the architectural character plays an important role in creating a memorable and enjoyable experience. In Milton, there is a strong architectural character stemming from the distinct periods of growth in the town including:

- Early growth period, from the mid to late 1800's, particularly around the town centre;
- Post World War II period, when several commercial buildings were replaced in the town centre, and further residential growth occurred throughout the town; and
- Modern growth, in more recent times following the re-orientation of the town and the major centre to a boutique village.

In the town centre, horizontal detailing tends to predominate in the walls and simple building masses of historic Milton buildings. Windows and door openings tend to be spaced well apart and have vertical proportions of 2 to 1. Larger openings are common for the ground floor of commercial buildings where proportions approach square. Use of windows tends to be simple and regularly spaced without undue complexity.

Consistency of architectural character within various precincts should be sought through materiality, colour and style.

5.9.1 Objectives

- i. New development is to respond to the character of the surrounding area with respect to architectural detailing, identifying the key elements and ensuring complementary design.

5.9.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
P19 Building design of retail and commercial buildings reflects the architectural character of prominent buildings in the Town Centre Precinct	A18.1 Key design elements of contributing and heritage buildings are considered and interpreted into new building designs including: <ul style="list-style-type: none"> • Parapet facades to the street frontage with generally flat roof behind; • Awnings to street frontages; • Use of solid materials; • Expression of vertical elements within the façade; • Provision of generous areas of glazed frontages at the ground floor, potentially broken up with small sections of wall; • Use of windows with vertical proportions and smaller sizes at upper levels;

Performance Criteria	Acceptable Solutions
	<ul style="list-style-type: none"> • Articulate and fragment building walls using colour and treatments to add visual interest, particularly on street frontages; and • Emphasis of built form on key corners and entrances to the town centre. <p>Refer Figure 13 for visualisation of these details.</p>
<p>P20 New or substantial modified residential buildings in the Established Residential Precinct reflect or complement the architectural character of original dwellings</p>	<p>A19.1 Key design elements are considered and interpreted into the new dwelling or modification to existing including:</p> <ul style="list-style-type: none"> • Roof forms have pitched roofs, generous verandahs and use of corrugated sheet roofing; and • Fencing has limited height (up to 1.2m) on the front and side boundary to the building line and is predominantly permeable (at least 50%) along the street frontage.
<p>P21 New or substantial modified residential buildings in the Emerging Residential Precinct complements the architectural character of original dwellings of the Established Residential Precinct</p>	<p>A20.1 Dwellings are of a character the complements the Established Residential Precinct, including elements such as verandahs and large overhangs (600mm+), and avoiding expansive wall areas (7.5m+) of single materials that are not broken up by windows/doors, articulation and variation in materials.</p> <p>A20.2 Key design elements consider and interpret the architectural character elements of the Established Residential Precinct (See P19)</p>

Chapter S6: Town of Milton

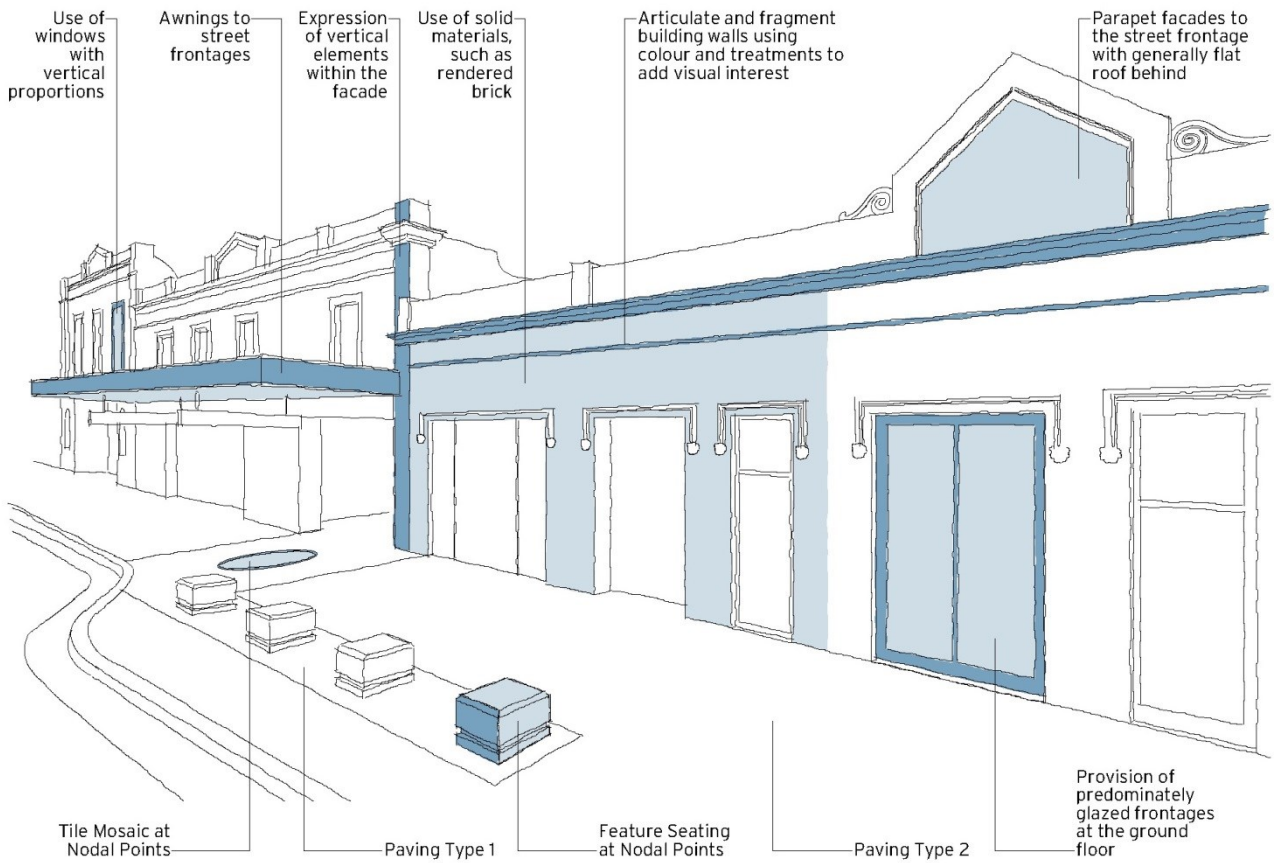


Figure 13: Town Centre Architectural and Built Form Character Visualisation

5.10 Awnings

Awnings, typically including balconies over the footways, were a significant component of the built form of the Princes Highway streetscape up until the mid-1900's when policies to improve safety saw the removal of many awnings from buildings adjoining the highway. Since this time, many awnings have been reinstated, typically cantilevered from buildings and acting only as weather protection devices (rather than as balconies). Awnings help to define active street frontages and improve the pedestrian amenity through protection during wet weather or through provision of shade during fine weather.

5.10.1 Objectives

- i. To provide awnings to all active street frontages in the town centre with consideration of heritage, views and design consistency

5.10.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
<p>P22 Awnings are to be provided to active uses to the street, pedestrian thoroughfares and public spaces in the Town Centre Precinct with design to consider impacts on heritage, visual intrusion on key views or view loss</p>	<p>A21.1 Awnings are cantilevered from buildings or provided with posts to the street frontage to best match surrounding awnings where applicable; and</p> <p>A21.2 Location of any posts associated with awnings are to consider proposed or future use of the footway and allowances made for reasonable pedestrian accessibility (generally being a minimum clearance of 2.4m); and</p> <p>A21.3 Awnings shall be setback a minimum of 0.5m from the street edge to avoid conflict with vehicles utilising the adjoining roadway.</p>

5.11 Active Frontages and Building Entrances

Within a town centre context, active frontages to streets and other public areas such as car parks and thoroughfares is an important component of ensuring interest, safety and amenity for pedestrians. The location and use of entrances to buildings and individual shops also provides an important component of activating streets and providing legible spaces for customers. Within heritage buildings, entrances should be located in the original main entrance location, activating the street as far as possible. Where dual entrances to shops are proposed, any street access should remain in permanent use and be designed as the primary access point rather than internalising entrances away from public spaces.

Similar principles apply for residential dwellings. Generally ensuring that dwellings address the street and are not dominated by parking and garages is important to ensure passive surveillance and a sense of community.

5.11.1 Objectives

- i. To maximise activation of streets, public spaces and car parking areas in retail and commercial areas;
- ii. To ensure building entrances are primarily focused to the street;
- iii. In situations where multiple frontages are proposed, from streets and thoroughfares for example, to ensure that additional non-street entrances are utilised as a secondary access points or alternatively activated through glazed walls; and
- iv. To ensure residential dwellings address the street in terms of windows/doors, pedestrian access and visual connection at street level.

5.11.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
<p>P23 Primary access to retail or other uses at the street level is located on the street frontage</p>	<p>A22.1 Primary access to individual retail or similar spaces is to be located on the street frontage wherever possible; and</p> <p>A22.2 Where applicable, secondary access is to be provided to other publicly accessible spaces such as thoroughfares; or</p> <p>A22.3 Where secondary access is not practical, walls to public spaces should including substantial areas of glazing to provide visual connection between internal and external areas.</p> <p>Note: Figure 14 identifies how these may apply to buildings with street frontages and arcades.</p>
<p>P24 Residential dwellings are designed to provide reasonable street activation</p>	<p>A23.1 Some living spaces, internal or external, are provided to the street frontage, with clear indication of pedestrian entry location and front door; and</p> <p>A23.2 Garages do not dominate the streetscape, being located to the side or rear of properties in the Established Residential Precinct and in accordance with Chapter G12 - Dwelling Houses, Additions and Ancillary Structures in other areas.</p>

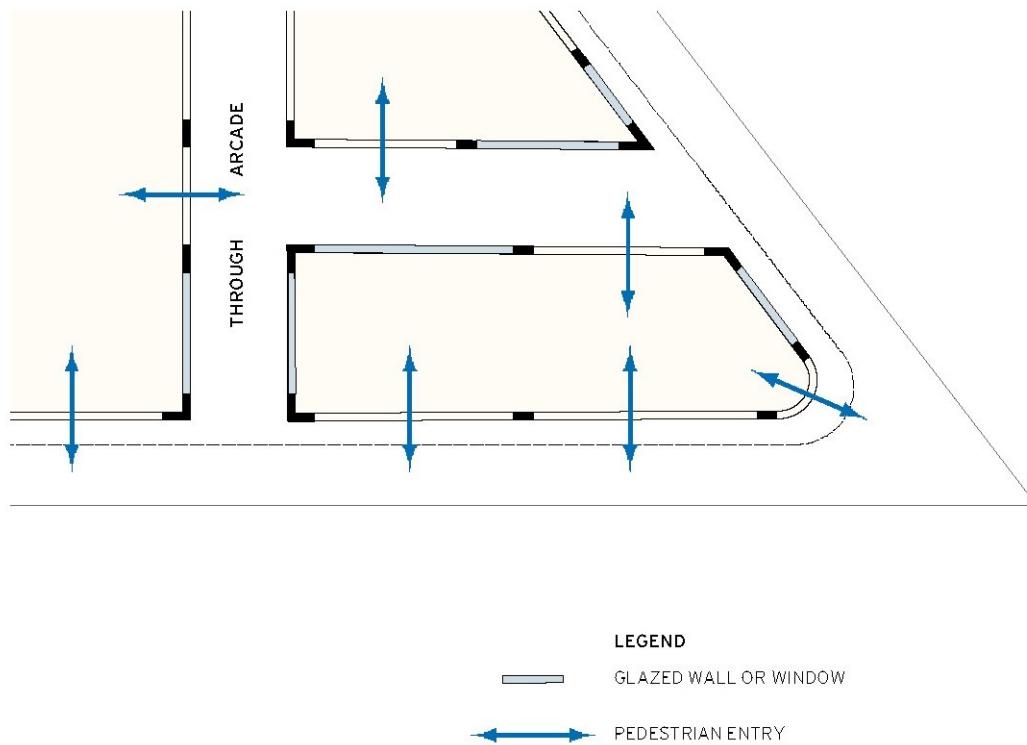


Figure 14: Acceptable Building Entrance Locations

5.12 Materials and Finishes

Milton's town centre commercial and civic buildings have a strong and relatively consistent character through the following use of materials and finishes:

- painted render with string courses;
- pediments and quoins picked out in different colours, with some sandstone quoins;
- corrugated iron roofing; and
- timber framed windows and doors.

These materials present robust and strong building elements which should be complemented in new development.

In established residential areas, buildings are generally:

- weatherboard or other timber or similar cladding; and
- corrugated iron roofing.

This basic material palette provides for a basis for new development as outlined in the Architectural Character section of this DCP.

5.12.1 Objectives

- i. On any individual building, to utilise a limited range materials and colours evident in Milton's town centre or established residential precincts as applicable;
- ii. To avoid the expansive use of any single material or blank walls through appropriate use of windows/doors, materiality and articulation;
- iii. To utilise high quality and durable materials and finishes; and
- iv. To avoid use of primary and incompatible colours.

5.12.2 Performance criteria and acceptable solutions

Performance Criteria		Acceptable Solutions	
P25	In the town centre area, utilise materials and styles that are consistent with existing commercial development	A24.1	For commercial and retail development, utilise materials that are consistent with, or have a design intent that reflects the traditional use of materials in the area, paying particular attention to the immediate surrounding buildings; and
		A24.2	Utilise a clear and minimal palette of materials with colours that are consistent with the heritage values of the area; and
		A24.3	Ensure blank facades are avoided, or where necessary and consistent with longer-term development, provide short term solutions such as the use of public art or murals to break down the building mass
P26	In residential areas, utilise materials and styles that are consistent with existing residential development	A25.1	Utilise wall cladding materials, such as timber, fibre cement sheeting or similar, and avoid the use of face brick in the Established Residential Precinct and minimise its use in the Emerging Residential Precinct; and
		A25.2	Utilise pitched roof designs with corrugated roof sheeting; and
		A25.3	Avoid use of primary colours, and utilise colour sets consistent with surrounding properties or other historical colour sets used in the Milton area.
P27	Materials used in development within the Rural Surrounds Precinct is cognisant of the visual impact of such	A26.1	Agricultural buildings and dwellings are designed to typically include pitched roofs consistent with the immediate surrounds, include overhangs and have corrugated roof sheeting; and

Performance Criteria	Acceptable Solutions
development and reflects rural design traits of the area	<p>A26.2 Differentiation is made between wall cladding and roofing material or other design features to avoid a humongous building mass; and</p> <p>A26.3 New agricultural sheds and ancillary buildings are to avoid pre-fabricated “colorbond” type construction unless the building would not be visible from the town and approaches.</p>

5.13 Landscaping

The landscape character of the area is characterised by the open rural areas punctuated by large landmark trees, including several large fig trees, pines and other screening and wind breaks associated with surrounding farms. A small but significant area of rainforest also exists on the periphery of the town centre, being a listed heritage item. Within the town, gardens and other landscape are more traditional, with hedges, formal gardens and exotic flowering plants generally associated with the cottages developed in the early period of the town.

5.13.1 Objectives

- i. To maintain and reinforce the key landscape elements of utilising larger landmark trees along the main ridge line and screen/corridor planting for agricultural and environmental needs;
- ii. To reinforce the grid street pattern through street tree planting; and
- iii. To assist the integration of emerging residential areas with the rural surrounds through a mix of onsite and mass planting in open spaces to soften the more dense built form.

5.13.2 Performance criteria and acceptable solutions

Performance Criteria	Acceptable Solutions
P28 Landmark tree planting is provided along the main ridge line to soften and complement built form, as identified in this DCP and demonstrated by way of a landscape plan prepared by a suitably qualified person.	<p>A27.1 Appropriate allowances are made in new development with frontages to the Princes Highway for the planting of landmark trees in key locations as identified in the <i>Town Centre Public Domain - Vegetation</i> plan at Figure 4; and</p> <p>A27.2 Selection of species is consistent with those identified in the <i>Town Centre Public Domain - Vegetation</i> plan at Figure 4.</p>
P29 Landscape planting assists to define areas of the town.	A28.1 Development in the Established Residential Precinct retains or re-establishes landscape

Performance Criteria	Acceptable Solutions
	garden to the street frontage utilising cottage gardens and consistent street tree planting; or
A28.2	Development and open spaces in the Emerging Residential Precinct include provision for deep planting to enable a substantial tree cover to be established over time; or
A28.3	Development in the Rural Surrounds Precinct utilises screening and hedging and other landscape treatments that provide a transition between the urban and rural area of the town.

6 Advisory Information

6.1 Summary of legislation, policies and other documents referred to in this chapter

Note: This section is not exclusive and you may be required to consider other legislation, policies and other documents with your application.

Council Policies & Guidelines	<ul style="list-style-type: none">• Contribution Plan
External Policies & Guidelines	<ul style="list-style-type: none">• Nil
Legislation	<ul style="list-style-type: none">• Shoalhaven Local Environmental Plan 2014
