

NOTE: This Chapter should not be read in isolation. You may need to consider other chapters of this DCP when preparing your application.



CHAPTER N7: NOWRA HOTEL SITE AND CIVIC PRECINCT



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1 Purpose

The purpose of this chapter is to provide **development** direction and planning control of the Nowra Hotel Site and Civic Precinct at Bridge Road Nowra.

Advisory Note: In addition to the provisions outlined in this Chapter, you must refer to the supporting maps: Chapter N7 – Maps 1-3 (contained in the one file).

2 Application

This Chapter applies to the Nowra Hotel Site and Civic Precinct at Bridge Road Nowra as shown in Figure 1 below.



Figure 1: Subject Land Map

3 Context

The provisions of the plan apply to the extent of any inconsistency with other Shoalhaven City Council **development control plans**, codes or policies.

This Chapter primarily relates to the potential to **develop** the northern portion of the subject land (Figure 2) for the specific intent of developing a hotel style accommodation, permanent living and associated amenity for visitors and residents alike. The remainder of the site which houses the existing **Council** Administration building, approved Entertainment Centre and associated vehicular access and parking are to remain unchanged.

You should consider the *Master Plan* of the site when designing your development.

4 Objectives

The objectives are to:

- i. Create a town focal point and to create a strong identifiable gateway to Nowra.
- ii. Create opportunities for mid to high level views of the Shoalhaven River and surrounds.
- iii. Provide new opportunities for the establishment of a tourist hotel and associated **development** that will provide enhanced amenity and economic stimulus within Nowra.
- iv. Improve existing streetscapes.
- v. Provide sufficient guidance relating to traffic, access and car parking for future **development** proposals so as to ensure a clear, safe, convenient and pleasant environment for both pedestrians and motorists.
- vi. Provide new and improved opportunities for quality apartment living within the Nowra area.
- vii. Provide guidance relating to the provision of suitable and attractive landscaping.
- viii. Promote a stronger pedestrian links to surrounding recreational areas and Nowra's business centre from the subject plan and civic precinct.
- ix. Provide guidance for future **development** proposals with respect to building design, height and bulk, layout and integration with existing **development**.

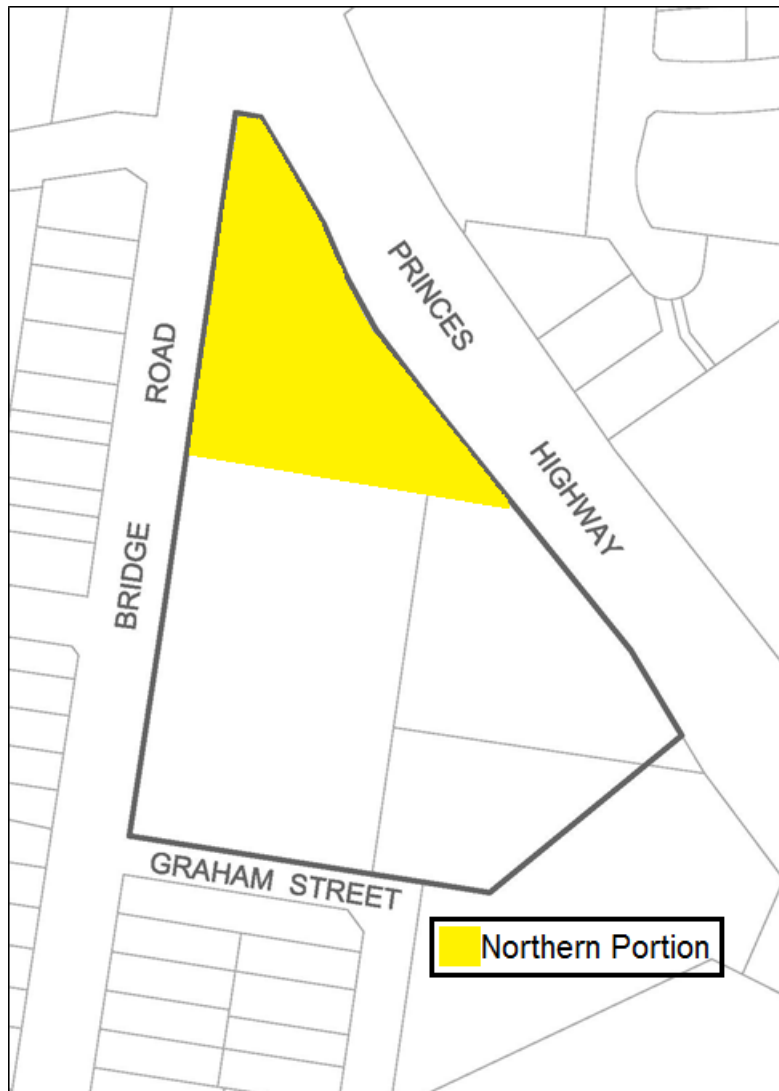


Figure 2: The northern portion of the site

5 Controls

The land subject of this **DCP** is one of the most visually prominent sites within the Nowra / Bomaderry area. The land has been previously identified as a suitable site for **development** that may provide the township of Nowra with an improved sense of identity. The site is ideal for **development** that can provide a defined entrance or gateway to Nowra as well as providing a new benchmark for tourist and permanent living environments.

The following principles should be read in conjunction with the diagrams contained in the attachments section of this document.

5.1 Built Form and Height

Building form and height have the potential to impact upon the amenity of a place. Building form and height may also reinforce, improve or compliment an area's, character and nearby existing **development**.

This section of the **DCP** provides guidance for future **development** proposals with respect to building form and height that should be adhered to.

5.1.1 Objectives

- To ensure that the form and height of buildings have a positive impact on the amenity of the area.
- To ensure that the form and height of buildings improve and compliment the area's character and nearby **development**.

5.1.2 Provisions

Performance Criteria	Acceptable Solutions
P1 The built form of new development is to create a 'gateway' development for the Nowra Town Centre.	<p>A1.1 Provide a 'gateway' statement to the intersection of the Princes' Highway and Bridge Road by introducing a taller 'residential/serviced apartment' building of up to 9 storeys in height.</p> <p>A1.2 Provide a lower scale 'hotel building' of up to 4 storeys that will appropriately frame the northern face of the public forecourt space created by the existing Council Administration building to the east and the Entertainment Centre to the south.</p>
P2 New development is to integrate with the existing Council Administrative Building, the Entertainment Centre.	A1 Provide other residential/serviced apartment buildings of up to 7 storeys aligned along the Princes' Highway frontage that when combined with the 'gateway' and 'hotel' building centre create an inner landscape courtyard space with activated frontages at its ground level.
P3 New buildings and uses are to create an attractive environment that is equally useable by visitors and locals and set a new benchmark for the Nowra Town Centre in terms of tourist accommodation and residential living.	<p>A2 Provide other residential/serviced apartment buildings of up to 7 storeys aligned along the Princes' Highway frontage that when combined with the 'gateway' and 'hotel' building centre create an inner landscape courtyard space with activated frontages at its ground level.</p> <p>A3 Provide a built form that enables the staging of future development.</p> <p>A4 Provide a built form of proportions and shape that will comply with the Residential Flat Code and SEPP65 for the relevant components.</p>

5.2 Character, Siting, Setbacks and Building Separation

The character of a place is attributed to the scale, siting and style of buildings and other physical features of the area. Building siting, separation and setbacks are important determinants of urban form. Inappropriate building separation, setbacks or building siting can lead to certain amenity problems such as lack of visual and acoustic privacy and reduced natural daylight. This section of the **DCP** provides guidance for future **development** proposals with respect to character, siting, setbacks and building separation that should be adhered to.

5.2.1 Objectives

- To ensure that the **development** of the area contributes to and enhances its character.
- To ensure that buildings are appropriately sited so as to minimise potential for general amenity problems.

5.2.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New buildings are to appropriately respond to the surrounding urban context including residential areas, street networks, open space and landscape in order to minimise negative environmental impacts and take full advantage of positive site attributes.	<p>New development proposals are to:</p> <p>A1.1 Be of contemporary nature and reflective of its uses,</p> <p>A1.2 Incorporate appropriately modulated forms that are compatible with the scale and proportions of the existing Council Administration Building and new Performing Arts Centre.</p> <p>A1.3 Provide a development that is integrated with landscape treatments and public open spaces.</p> <p>A1.4 Provide clear definition of relationships between proposed individual buildings and the Council Administration Centre and new Performing Arts Centre, including the new Civic Forecourt.</p> <p>A1.5 Provide appropriately defined pedestrian access points and circulation paths.</p>
P2 Buildings are to be sited in such a way that potential visual and acoustic amenity issues are minimised.	A2.1 Sited so as to minimise adverse environmental impacts such as noise, overshadowing and winter winds and to maximise desirable attributes such as orientation to views, solar access summer

		breezes.
P3 The potential for unacceptable overshadowing is to be minimised.	A3.1	Be consistent with setbacks and separation indicated on diagrams <i>Supporting Map 1</i> contained in the attachments to this document.

5.3 Landscape

Landscaping can enhance the appearance and amenity a place and is fundamental to the design of most **development** proposals. **Development** proposals incorporating suitable landscaping result in better aesthetic quality and amenity for occupants and adjoining properties.

This section of the **DCP** provides guidance for future **development** proposals with respect to landscaping.

You should also refer to *Chapter G3 Landscaping Design Guidelines* of this DCP.

5.3.1 Objectives

- To ensure that landscaping is used to enhance the appearance and amenity of the public domain.
- To ensure that **development** proposals incorporate landscaping that results in quality outdoor spaces for casual users and permanent occupants.

5.3.2 Provisions

Performance Criteria	Acceptable Solutions
P1 Landscaping is to enhance the appearance and amenity of development through the implementation of excellent landscape design.	A1.1 Landscape treatments are to be integrated with the Princes Highway landscape buffer zone.
P2 Proposals are to include sufficient landscaping to ensure quality areas of open space are provided as part of new development proposals.	A2.1 Landscape treatments to be integrated with the desired streetscape to Bridge Road including street trees and road improvements and vehicular access points.
P3 Landscaping is to integrate with that landscaping existing in the current public domain.	<p>A3.1 Integrate hotel port cochere' vehicular entry road with proposed Civic Forecourt landscape treatments.</p> <p>A3.2 Provide high quality, integrated landscape treatment to pedestrian circulation paths and courtyard space.</p> <p><i>See Supporting Map 2.</i></p>

5.4 Interface between Buildings and the Existing and Proposed Public Domain

It is important to ensure that new **development** integrates well with and contributes to the existing and proposed public domain. The public domain generally incorporates shared areas and spaces, the structures that form and relate to those spaces and the infrastructure that serves them.

This section of the **DCP** provides guidance for future **development** proposals with respect to creating a suitable interface between buildings and the existing and proposed public domain.

5.4.1 Objectives

- To ensure that new **development** both integrates with and enhances the existing public domain.

5.4.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New development proposals are to integrate in a positive manner to the existing and proposed public domain.	Development proposals are to: A1.1 Maximise the provision of activated edges including street frontages and landscaped courtyard frontages.
P2 Areas of public and private space are to be clearly defined.	A2.1 Ensure clear definition of entry/exit points to all building including the provision of appropriate safety and security measures.
P3 Potential for adverse overshadowing of civic spaces is to be minimised.	A3.1 Minimise adverse shadow impacts between 9:00am and 3:00pm for mid-winter on the Civic Forecourt Space to the Performing Arts Centre and surrounding residential development.
P4 Development proposals are to provide opportunities for the casual surveillance of public places.	A4.1 Provide over-viewing of courtyard space and pedestrian circulation areas.
P5 Development proposals are to enable opportunities for the provision of suitable and effective pedestrian connectivity.	A5.1 Provide appropriate through site links and provision for future pedestrian connection to surrounding areas. A5.2 Create a strong visual axis between the Hotel entry and Performing Arts Centre entry through The Civic Forecourt.

5.5 Views

The land subject of this development control plan provides excellent opportunities for obtaining views of the Shoalhaven River and Barrengarry Range to the north.

This section of the DCP provides guidance with respect to obtaining views.

5.5.1 Objectives

- To ensure that new development obtains available views of the Shoalhaven River and Barrengarry Range.

5.5.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New development is to be designed such that opportunities available for obtaining views from the site are maximised.	A1.1 New development proposals are to be of a height and orientation so as to maximise views of the Shoalhaven River and Barrengarry Range.

5.6 Privacy

It is important that future development (particularly residential) is designed and orientated such that the privacy of its inhabitants (visual and acoustic) is protected.

This section of the DCP provides guidance with respect to the mitigation of potential privacy issues.

5.6.1 Objectives

- To ensure that development is designed and oriented such that potential visual and acoustic privacy are minimised.

5.6.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New development, particularly residential development, is to be designed in such a manner that reasonable levels of visual and acoustic privacy can be ensured.	A1.1 New development is to be designed so as to maximise visual and acoustic privacy between private living spaces.
P2 New development is to provide reasonable opportunities for views from living areas without affecting visual privacy.	A2.1 Residential development is to be designed in accordance with the provisions of State Environmental Planning Policy 65 and the Residential Flat Code for the residential components of the development.
P3 New development is to be designed such that a reasonable level of amenity with respect to noise for residential apartments	

is afforded to inhabitants.

5.7 Solar Access and Energy Efficiency

Correct building orientation can significantly improve the amenity of a site and/or buildings. Internal temperatures and demand for heating and cooling can be greatly affected by the amount of solar access available to buildings.

This section of the DCP provides guidance for future development proposals with respect to solar access and energy efficiency.

5.7.1 Objectives

- To ensure that new development is designed and orientated having regard to solar access and energy efficiency considerations.

5.7.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New buildings are to be designed and oriented so as to achieve appropriate levels of solar access and energy efficiency.	A1.1 Future appropriate levels of solar access are to be achieved in accordance with the provisions of: <ul style="list-style-type: none">• State Environmental Planning Policy 65 and the Residential Flat Code for the residential components of the development.• Environmentally Sustainable Design (ESD) principles.• BASIX where applicable.• Excessive overshadowing of private open spaces and private living spaces particularly in residential areas should be avoided.

5.8 Mitigation of Environmental Impacts

5.8.1 Noise attenuation

The major source of noise affecting the site comes from the Princes Highway which adjoins its eastern boundary. Certain measures should be taken so as to minimise the potential for noise to impact upon the future amenity of the site.

This section of the DCP provides guidance for future development proposals with respect to noise attenuation.

5.8.2 Objectives

- To ensure that the issue of noise attenuation is given appropriate consideration and new development proposals are appropriately designed and constructed so as to protect the amenity of the development site.

5.8.3 Provisions

Performance Criteria	Acceptable Solutions
P1 Development proposals are to reasonably ensure that noise from the Princes Highway does not adversely impact upon amenity.	A1.1 A quantities road traffic noise assessment is to be undertaken with consideration to the AS 367 -1989, Acoustics -Road Traffic Noise Intrusion -Building Siting and Construction.

5.9 Flooding

It is important to ensure that the land can be provided with flood free access and that all habitable floors are constructed above the flood planning level. It is also important to ensure that building materials used are flood compatible.

This section of the DCP provides guidance for future development proposals with respect to flooding.

5.9.1 Objectives

- To ensure the provision of flood free access to all new development.
- To ensure new buildings can withstand the force of flowing water.
- To ensure the maintenance of residential amenity during extended wet periods.

5.9.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New development is to be provided with flood free access.	A1.1 Whilst the subject site is nearby to the other flood affected lands, it has been established that the land subject of this development control plan is not affected by flooding. Therefore no specific conditions should affect the development of land with respect to flooding.
P2 New buildings must withstand the force of flowing water during flood events.	
P3 New development is to ensure that a reasonable level of residential amenity during can be maintained during extended wet periods.	

5.10 Soil, Water and Site Management

It is important that all discharges from the site do not adversely impact upon the environment (built or natural). Discharges potentially harmful to the environment from building sites include polluted storm water and building waste.

This section of the DCP provides guidance for future development proposals with respect to soil, water and site management.

5.10.1 Objectives

- To help maintain the health of the environment – particularly streams and rivers.
- To minimise the production of waste generated during construction .

5.10.2 Provisions

Performance Criteria	Acceptable Solutions
P1 Development proposals are to incorporate measures that maintain the health of the environment – particularly streams and rivers.	A1.1 Any proposed development of the site will need to comply with NSW and SCC legislative requirements for water, soil and waste management, specifically: <ul style="list-style-type: none">• A sedimentation and erosion control plan prepared by a suitably qualified person• A Waste Minimisation and Management Plan (WMMP) prepared in accordance with <i>Chapter G7 Waste Minimisation & Management Controls</i> of this DCP.• Maintenance of all stormwater drainage pipes and systems to ensure efficient discharge of stormwater in accordance with the approved stormwater drainage plan.
P2 The production of waste generated during construction is to be minimised through the implementation of waste minimisation principles.	

5.11 Traffic, Transport and Access

5.11.1 Public transport

Public transport within the Nowra area consists of a local bus service, the hub of which is located at Stewart Place. The bus service provides a basic level of access to key destinations of the Nowra CBD and connections to the Bomaderry train station.

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Trains run throughout the week from Bomaderry and Berry Stations, with access to Wollongong and Sydney requiring a stop to change electronic trains at Kiama. The train services operate at two hour intervals from early in the morning to late at night with extra commuter train services operating during the week. Taxis provide service 7 days a week to most of the Shoalhaven.

This section of the DCP provides guidance for future development proposals with respect to public transport.

You should also refer to *Chapter 21 Car Parking and Traffic* of this DCP.

5.11.2 Objectives

- To ensure that future development can cater for and encourage the use of public transport systems.
- To reduce car dependency for commuting to and from the site.

5.11.3 Provisions

Performance Criteria	Acceptable Solutions
P1 Development proposals are to cater for and encourage the use of public transport systems.	A1.1 Future development proposals are to make provision for the parking of large rigid buses in accordance with applicable design guidelines.
P2 Development proposals are to contribute to an overall citywide aim of a reduction in car dependency.	A2.1 A designated area should be made available for the collection and setting down of taxi passengers. A2.2 Future development proposals are to take into consideration the adopted Shoalhaven Integrated Transport Strategy.

5.12 Pedestrian/Cyclist Access

It is important to ensure that any proposed development of the site creates opportunities for pedestrian access and integrates well with existing pedestrian infrastructure.

5.12.1 Objectives

- To ensure that new development proposed for the site is safely accessible for pedestrians and cyclists including wheelchair and stroller users.

5.12.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New development proposals are to be safely accessible for pedestrians and cyclists including wheelchair and stroller	A1.1 Future development proposals are to provide pedestrian access to residential development in accordance with the provisions

users.

of State Environmental Planning Policy 65, the Residential Flat Code and relevant Australian Standards.

A1.2 Future development proposals are to take into consideration the Shoalhaven Integrated Transport Strategy.

A1.3 The design and layout of proposed development is to make provision for a future cross-highway pedestrian connection.

5.13 Access, Traffic Management and Parking

The provision of suitable road access to the site will be most important in consideration of its future development. Access to the site will be restricted to Bridge Road as no new access points off the Princes Highway will be permitted. Bridge Road is subject to a high number of vehicle movements – especially in the peak hours. The development of this site will increase traffic movements in the immediate area, which is already very busy.

The general requirements for parking on the site involve provision of sufficient capacity for passenger vehicles, coach parking, as well as passenger drop-off (e.g. taxi, coach etc) associated with the development.

This section of the DCP provides guidance for future development proposals with respect to road access.

5.13.1 Objectives

- To provide a clear set of principles relating to road access that should be noted when considering the development of the site.
- To ensure that future development of the site is provisioned with access that is legible, co-ordinated and user friendly.
- To ensure that future development has acceptable impact on existing traffic flows.

5.13.2 Provisions

Performance Criteria	Acceptable Solutions
P1 New development is to be provisioned with access that is legible, co-ordinated and user friendly.	A1.1 Site access should reflect the following access principles – <ul style="list-style-type: none">• Vehicular entries off Bridge Road should be minimised and not permitted in certain areas as shown indicatively on the DCP map relating to access. <i>See Supporting Map 3.</i>• Access is to be integrated with the existing Council administration building and cultural centre.• The entry off Bridge Road should provide a high quality entry to the site and porte

	<p>cochre address for the hotel.</p> <ul style="list-style-type: none">• The design of access is to comply with Shoalhaven City Council's relevant design Standards and RTA Road Design Guidelines.• Clearly defined vehicular service access points are to be provided with provision of loading and storage facilities appropriate for the development uses.• Parking facilities will be designed to ensure that circulation and queuing criteria are met, and that vehicle / pedestrian conflict points are minimised and addressed in design.
<p>P2 New development is to have acceptable impact on existing traffic flows.</p>	<p>A2.1 The design of civil works will comply with the following;</p> <ul style="list-style-type: none">• Shoalhaven City Council relevant policies and design standards for civil engineering.• AUSTRROADS Guide to Traffic Engineering – Parking.• RTA Road Design Guidelines.
<p>P3 New Development to accommodate all parking requirements within the subject.</p>	<p>A3.1 Future development proposals will need to be supported with a parking strategy and detailed traffic impact study that demonstrate adequate provision of parking facilities contained within the subject site and with reference to Shoalhaven City Council's relevant design standards and RTA Design Guidelines.</p>

5.14 Public domain enhancement and integration

It is important to integrate new buildings and areas of open space to the existing public domain. This section of the DCP provides guidance for future development proposals with respect to public enhancement and integration.

5.14.1 Objectives

- To ensure all new buildings and areas of open space integrate with and enhance the public domain.
- To ensure new development incorporates pedestrian linkages with existing public spaces and associated infrastructure.
- To ensure new landscaping enhances the visual presentation and pedestrian environments associated with the site.

5.14.2 Provisions

Performance Criteria	Acceptable Solutions
P1 Development proposals are to demonstrate that new buildings and areas of open space integrate with and enhance the public domain.	A1.1 Areas of public space are to be linked to the public street and reserve areas. A1.2 Street trees are to be retained and or planted along Bridge Road that enhance its appearance and its appeal to pedestrians.
P2 New development is to incorporate new pedestrian linkages with existing public spaces and associated infrastructure.	A2.1 The design and layout of future development is to make provision for a future cross-highway pedestrian connection.
P3 New landscaping is to enhance the visual presentation and pedestrian environment associated with the site.	A3.1 Landscape treatments to the Princes Highway is to be integrated with existing treatments as part of future development proposals.

6 Advisory Information

6.1 Other legislation or policies you may need to check

Note: This section is not exclusive and you may be required to consider other legislation, policies and other documents with your application

Council Policies & Guidelines	<ul style="list-style-type: none"> • Master Plan for the Nowra Hotel and Cultural Centre Precinct by Shoalhaven City Council, Cox Humphries Moss, GHD and Envirolinks Design (December 2005).
External Policies & Guidelines	<ul style="list-style-type: none"> • Residential Flat Design Code • Building Code of Australia • AS 367 -1989, Acoustics -Road Traffic Noise Intrusion -Building Siting and Construction • Shoalhaven Integrated Transport Strategy • AUSTRROADS Guide to Traffic Engineering – Parking • RTA Road Design Guidelines
Legislation	<ul style="list-style-type: none"> • Shoalhaven Local Environmental Plan 2014 • State Environmental Planning Policy (Building Sustainability Index: BASIX) 2004 • State Environmental Planning Policy 65 – Design Quality of Residential Flat Development